

**REPORT FOR AMENDMENT NO. 2
TO THE SOUTHWEST AREA
ROAD AND BRIDGE BENEFIT DISTRICT**

JUNE 2007

COUNTY OF RIVERSIDE . TRANSPORTATION DEPARTMENT
4080 Lemon Street, 8th Floor
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SOUTHWEST AREA ROAD AND BRIDGE BENEFIT DISTRICT

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I. EXECUTIVE SUMMARY

The Southwest Road and Bridge Benefit District (the “District”) was formed to fund specific, regional road and bridge improvements determined to provide a benefit to the developing properties within portions of the southwest areas of Riverside County. The procedures for the formation and amendment of a Road and Bridge Benefit District were established by Resolution No. 85-92, *Rules and Regulations for Administration of Road and Bridge Benefit Districts*, as adopted by the Board of Supervisors on April 2, 1985. These Rules and Regulations require that a District be examined annually to determine whether adjustments are needed to the boundaries, the designated facilities and/or the fee schedule in response to inflationary and other cost adjustments affecting the estimated construction costs.

Additionally, on February 10, 2003, the Transportation Uniform Mitigation Fee (the “TUMF”) became effective. A few of the transportation facilities listed under the District partially overlap with some of the facilities contained within the TUMF Program. To eliminate the overlap in fees and to preserve the funding mechanism established by the District, the District fee charged to new development generates a credit against the TUMF fees that is applied toward any overlapping facilities. After the approval of this amendment, TUMF credits will be discontinued for eight(8) of the nine(9) facilities listed on Table 1. The TUMF Program will supplement several other facilities in the District for project costs that are expected to exceed the amount collected in the TUMF.

Table 1

	FACILITIES TO BE REMOVED	ZONE(S)	RBBB Deleted Budgets
	Name of Facility		
1	Baxter Road Interchange @ I-15	A	\$ 9,759,960
2	Bundy Canyon Interchange @ I-15	A	\$ 9,759,960
3	Clinton Keith Road Interchange @ I-215	D	\$ 2,293,591
4	Los Alamos Road Interchange @ I-215	D	\$ 2,654,709
5	Central Street(Palomar to I-15)	A	\$ 1,206,900
6	Baxter Road(I-15 to 6,000 to E'ly of I-15)	A	\$ 1,830,600
7	Palomar Street(Mission Trail to City of Murrieta)	A	\$ 3,982,770
8	Washington(City of Murrieta to Keller Rd.)	D	\$ 13,891,150
9	Clinton Keith Bridge(Completed)	A, C	\$ 294,840
	TOTAL		\$ 45,674,480

This request will not only remove TUMF overlapping facilities but will also add two(2) new facilities to meet the traffic demands of the Southwest area shown in Table 2, remove one(1) completed bridge, and will expand two(2) existing facilities to include funding for six(6) lanes on Winchester(from Auld to Keller) and Bundy Canyon(from Mission Trail to Sunset Avenue). The District currently funds lanes three(3) and four(4) on Winchester Road, and only two lanes on Bundy Canyon from Mission Trail to Sunset Avenue.

Table 2

		Proposed RBBB Budgets	Zone
	NEW PROPOSED FACILITIES		
	A. Roads/Drainage Improvements		
1	Keller(Rte. 79 to Washington)	3,362,688	D
	B. Bridges		
2	La Estrella Bridge	5,000,000	A
	TOTAL	8,362,688	

II. BACKGROUND

Pursuant to Section 66484 of the California Government Code and Chapter 16.28 of the Riverside County Code, a road and bridge benefit district may be established by the Board of Supervisors of the County of Riverside (respectively, the "Board" and the "County") to fund the estimated construction cost of major roads and bridges that are identified in the current Circulation Element of the County of Riverside's Comprehensive General Plan. The Southwest Area Road and Bridge Benefit District (the "Southwest District") was originally established in 1993 to fund the estimated construction costs of certain identified road and bridge facilities needed for the projected development of unincorporated areas in the southwest portion of the County. At the conclusion of a noticed public hearing on August 28, 2001, which was ratified by the adoption of Resolution No. 2001-317 on September 25, 2001, the Board reestablished the Southwest District in accordance with the terms of the Settlement Agreement (defined herein below). Subsequent to that, the Board adopted an amendment through Resolution No. 2002-238 on June 25, 2002, that expanded the boundaries of Zone D.

The Board by the adoption of Resolution No. 93-056 on January 26, 1993, established the Southwest District. This action became the subject of a legal challenge [*Cecelia Webster v. County of Riverside, et al.* (RSC Case No. 217120)] that alleged violations of CEQA. The suit was subsequently settled pursuant to the terms of a settlement agreement dated June 7, 1994 (the "Settlement Agreement"). In compliance with the Settlement Agreement:

1. The County prepared a conceptual alignment study for the segment of Clinton Keith Road between Interstate 215 and State Route 79 ("Winchester Road") and processed a general plan amendment ("General Plan Amendment No. 409") to fix the alignment of said road.
2. The County prepared a draft Environmental Impact Report ("EIR No. 398") addressing the general plan amendment and the establishment of the Southwest District.
3. General Plan Amendment No. 409 was adopted and EIR No. 398 was certified by the adoption of Resolution No. 2000-335 by the Board on December 19, 2000.

4. The County reestablished the Southwest District on September 25, 2001 and vacated and set aside Resolution No. 93-056 and the Negative Declaration for Environmental Assessment No. 36392 by the adoption of Resolution No 2001-317.

The Southwest District is located in the southwest portion of the County and comprises three Zones consisting of unincorporated territory. Zone A is located northwest of the City of Murrieta (the "City"); Zone C is southwest of the City and Zone D is immediately east of the City's boundaries, all as shown in Exhibit A.

The projected development for the areas comprising the three Zones will significantly impact traffic circulation within the Southwest District. Either existing facilities will have to be improved or additional major roads and bridges will have to be constructed to meet these increased traffic demands. The Southwest District provides a funding mechanism to pay the estimated construction costs of the needed facilities as identified and defined in this Report.

There is also a need to move forward with construction of the interchange improvements at Clinton Keith/I-15 in order to serve the existing traffic demands and expected continued growth in the County and City of Murrieta. On May 25, 2004 the Board of Supervisors adopted an aggressive strategy, also known as "Clinton Keith Road/I-15 Policy Area," and the use of fees from development revenues to advance funding for this activity. This project is partially funded by Zone A of the District and fees collected from this zone will be required at final map recordation. Fees will not be deferred to building permit issuance in an effort to support the Board's desire to provide advance funding for this facility. Collection of the fees at the time of map recordation, rather than upon building permit issuance will provide for an earlier accumulation of revenues needed to construct this facility.

Several property owners in the District will be participating in the Community Facilities District(CFD) 07-2 that will provide funding for the construction of Clinton Keith Road including the bridges over Warm Springs Creek, and the wildlife crossing. A credit will be calculated for participants to be applied against the District once the CFD is approved. The credit will be calculated based on the contribution made by the property owners to the CFD to finance this project. Participants will receive credits against the District fees upon recordation of a final map or issuance of a building permit for participants in the District, except for those in Zone A that will have to pay fees at the time of recordation.

The District has been designed in a manner to distribute the cost of the construction of the improvements on an equitable basis among benefiting properties. The fee levels for this District are determined based on the estimated improvement costs and projected future development potential of each zone. The fees are a one-time charge paid to the Transportation Department prior to recordation of a final tract or parcel map or prior to the issuance of a building permit. Properties that have already been developed are not subject to District fees.

The Transportation Department has prepared an Initial Study for Environmental Assessment No.41164 to evaluate the: addition of the two(2) new facilities; expansion of Winchester and Bundy Canyon(from Mission Trail to Sunset Avenue); removal of nine(9) facilities; increased and/or decreased cost estimates for several facilities needed to reflect additional design and engineering information; land use assumptions used to determine the fee schedule for the District updated to reflect the Riverside County Integrated Project; and fees needed to be adjusted to be consistent with the aforementioned changes. All other identified facilities were previously analyzed for impacts and mitigation measures under the original adoption, subsequent amendments to the Southwest District, and are also evaluated at the project level.

III. CHANGES IN LAND USE PROJECTIONS

The Transportation Department's method for projecting the number of residential dwelling units assumes that undeveloped portions of approved specific plans will develop at 85% of the specific plan's maximum residential dwelling unit allocation. For undeveloped residential property not subject to an approved specific plan, the Transportation Department assumes that the property will achieve a build out equivalent to the midpoint density allowed by the property's Southwest Area Land Use Plan designation. For example, if a property has a Southwest Area Land Use Plan designation of Medium Density Residential (2 to 5 dwelling units per acre), a midpoint density of 3.5 dwelling units per acre is assumed. Adjustments to this methodology have been made when a final subdivision map is recorded that conforms to the density range allowed by an approved specific plan or the Southwest Area Plan. Within these subdivisions, one dwelling unit per undeveloped lot has been assumed. For "Commercial", "Office Commercial", and "Industrial" land use designations, undeveloped net acreage is used to estimate the development projection of each use designation. For the French Valley Airport property in Zone D, development projections are based on the County's airport development plans administered by the Riverside County Economic Development Agency. These same methodologies are the methodologies employed by the Transportation Department in establishing the current development projections for the Menifee Valley Road and Bridge Benefit District and the Mira Loma Road and Bridge Benefit District, and Scott Road and Bridge Benefit District (the "Existing Districts") that are the three established districts that have been approved by the Board.

Zone A of the District is within the "Highway 79 Policy Area" established by the Board of Supervisors and included in the Riverside County Integrated Project(RCIP), which calls for a 9% reduction below the General Plan Land Use mid-point to reduce traffic impacts in the Policy Area. The midpoint range of 3.5 dwelling units per acre for a future build-out is reduced by the 9%, and equates to 3.19 dwelling units per acre for a future build-out within the Highway 79 Area.

IV. FACILITIES IDENTIFIED FOR FUNDING

The typical street improvement sections, Exhibit B, are based on the current Riverside County Transportation Department Improvement Standards for Expressway, Urban, Arterial, and Secondary roadway designations. Exhibit B also provides typical sections for bridges that are proposed in this amendment. The District will fund improvements to roadways based on the County standard curb to curb only. The construction of roadway frontage improvements, e.g., (sidewalks, curb and gutter, and landscaping) will be the responsibility of the adjoining property owners. Right of Way costs will be eligible for reimbursement for only one(1) project in the District that will be constructed by the Department, and that is Clinton Keith Road(Menifee Rd. to Hwy 79). The District will not cover Right of Way costs for any other projects. Unless otherwise specified herein, the District will not fund activities that do not result in ultimate improvements such as tapers or interim projects.

The facility improvement costs are estimated costs and actual costs of facilities may be higher or lower than indicated. Facilities' budgets include a factor of 45% that is intended to cover: construction cost contingency; design engineering; contributions to the Multi-Species Habitat Conservation Plan for coverage of facilities under the plan, preliminary survey; construction inspections and management; and District Administration costs. A 5% administration cost will be maintained in the District Fund for the management and administration of the District.

At the time Resolution No. 93-056 was adopted, the City of Murrieta had incorporated and had approved a road and bridge benefit district for the territory within its boundaries. The City's district identified for funding a number of freeway interchanges on both Interstate 15 and Interstate 215 that would also benefit property owners within Zone D of the Southwest District. The County and the City entered into an agreement that specified the percentages that their district and the Southwest District would contribute to each of the shared facilities. Clinton Keith Rd Interchange at I-15; Murrieta Hot Springs at I-215, Winchester Road, and Clinton Keith Road from Menifee Rd. to SR79 are facilities funded by the Southwest District and the City of Murrieta. The percentages identified in this Report for contribution by the District toward the estimated construction costs of these facilities are based in part on this agreement. The estimated construction costs of the shared facilities utilized in this Report are consistent with the current cost figures used by the City. A further breakdown of how the construction cost estimates were derived is provided in Exhibit C of this Report.

The following is a listing of each facility identified for funding within the Southwest District. The list states the current estimated construction cost for each facility and includes a comparison to the cost estimate approved on August 28, 2001 (For Zones A and C) and on June 25, 2003 (for Zone D). A facility description follows each list that identifies each facility's location and/or limits, its cost sharing percentage, if any, its current Circulation Element designation, and all design considerations affecting the facility's construction cost estimate. Exhibit C further provides facility summary details and comparisons by zone. Exhibit D provides a comprehensive listing of all the facilities in the District and program eligible reimbursement rates for each facility.

A. Cost Estimates-District Facilities for all Zones -Table 3

SOUTHWEST FACILITY SUMMARY TOTAL					
			COMPARISON	COMPARISON	
	Name of Facility	Proposed RBBB Budgets	Current RBBB Budgets	Variance (Incr/decre)	Variance %(Incr/decre)
A. Interchanges					
1	Clinton Keith Road Interchange @ I-15	16,300,000	6,636,773	9,663,227	146%
2	Los Alamos Road Interchange	DELETE	2,654,709	-2,654,709	-100%
3	Murrieta Hot Springs Rd @ I-215 Interchange	2,010,690	2,654,709	-644,019	-24%
4	Clinton Keith Road Interchange @ I-215	DELETE	2,293,591	-2,293,591	-100%
5	Baxter Road Interchange @ I-15	DELETE	9,759,960	-9,759,960	-100%
6	Bundy Canyon Interchange @ I -15	DELETE	9,759,960	-9,759,960	-100%
B. Roads/Drainage Improvements					
7	Bundy Canyon(Mission Trail to Corydon)	1,000,000	1,182,912	-182,912	-15%
8	Bundy Canyon Road(Mission Trail to Sunset Ave)	20,000,000	8,077,413	11,922,587	148%
9	Clinton Keith Road (City of Murrieta to Zone C Boundary)	2,000,000	1,206,900	793,100	66%
10	Winchester Road/State Highway 79(Auld to Keller Rd.)	10,576,000	4,682,470	5,893,530	100%
11	Clinton Keith Rd.(Menifee Rd to Hwy 79)	22,800,000	19,282,370	3,517,630	18%
12	Benton Rd.(Hwy 79 to Washington)	3,000,000	Included #11	3,000,000	100%
13	Keller(Rte. 79 to Washington)	3,362,688	New	3,362,688	100%
14	Central Street	DELETE	1,206,900	-1,206,900	-100%
15	Baxter Road	DELETE	1,830,600	-1,830,600	-100%
16	Palomar	DELETE	3,982,770	-3,982,770	-100%
17	Washington	DELETE	13,891,150	-13,891,150	-100%
C. Bridges					
18	La Estrella Bridge	5,000,000	New	5,000,000	100%
19	Clinton Keith Road Bridge @ Warm Springs Creek East	Included # 11	Included # 11		
20	Clinton Keith Road Bridge @ Warm Springs Creek West	Included # 11	Included # 11		
21	Washington St at French Valley Stream	3,000,000	3,000,000	0	0%
22	Clinton Keith Bridge(Built)	DELETE	294,840	-294,840	-100%
D. Landscaped Medians					
23	Benton Rd. Landscaped Median (SR79 to Pourroy Road)	1,043,561	Included #12	1,043,561	100%
TOTAL		90,092,939	92,398,027	(2,305,088)	-2%
TOTAL REVENUES RECEIVED		26,626,803	2,881,288	23,745,515	824%
TOTAL REVENUES NEEDED		63,466,136	89,516,739	-26,050,603	-29.10%

*Reported revenues for this proposed amendment are through the end of March 31, 2007.

B. State Highway Interstate/Overcrossing Improvements:

1. Interchange of Clinton Keith Road @ I-15 Freeway This project will be funded by Zone A at 97.5%, and Zone C at 2.5%. The City of Murrieta will contribute \$2,000,000, TUMF will provide \$16.7 million, and the Southwest District will provide \$16.3 million towards the approximate total cost of \$35,000,000.
2. Interchange of Murrieta Hot Springs Road @ I-215 Freeway This project will be funded by Zone B that falls under the jurisdiction of the City of Murrieta at 72.8%, and Southwest District Zone D at 27.2% in the amount of \$2,010,690.

C. Roadway and Bridge Improvements:

1. Bundy Canyon (Mission Trail to Corydon) This project will be funded by Zone A at 100%. This proposal will fund four(4) lanes, and is designated as an Urban Arterial.
2. Bundy Canyon (Mission Trail to Sunset Ave) This project will be funded by Zone A at 100%. This proposal will add six(6) lanes, and is designated as an Urban Arterial. Most of this project will be constructed by the Department.
3. Clinton Keith Road(From Southerly Zone C Boundary to City of Murrieta) This project will be funded by Zone C at 100%. This proposal will fund the 3rd and 4th lanes, and is designated as a Mountain Urban Arterial.
4. Winchester Road / State Highway 79 (From 800' (245 meter) north of Auld Road to Keller Road) The Transportation Department built 7227 linear feet of the 3rd and 4th lanes, and two(2) intersection improvements. These improvements incurred expenditures beyond the revenue that this facility had collected. As a result, the fees established for this district include repayment of these costs and funding to cover lanes one(1), two(2), three(3), four(4), five(5), and six(6). This project will be funded by Zone D at 100% and the TUMF program will supplement the project. This facility is designated as an Expressway. Funding is also included to cover the raised median.
5. Clinton Keith Road (From Highway 79 to Menifee Road) This project will be funded by Zone D at 100%. This proposal will add six(6) lanes, and is designated as an Urban Arterial. Estimates for this project include funding for the construction of east and west bridges over Murrieta Creek, and a wildlife crossing. The project will include supplemental funding from the TUMF program and will be constructed by the Department. This amendment includes funding for right-of-way costs to cover expenses incurred by the Department for the construction of this project.

6. Benton Road (From Highway 79 to Washington) This project will be funded by Zone D at 100%. This proposal will reconstruct the middle two(2) lanes, and is designated as an Urban Arterial. The project will include supplemental funding from the TUMF program that will fund the outer lanes.
7. Keller(Highway 79 to Washington) This project will be funded by Zone D at 100%. This proposal will add the middle four(4) lanes, and is designated as a Secondary roadway.

Bridge Improvements

8. La Estrella Bridge This project will be funded by Zone A at 100%.
9. Washington Street Bridge at French Valley Stream This improvement will be funded by Zone D at 100%. This bridge will be constructed for an Urban Arterial.
10. Clinton Keith Bridges(East & West) This improvement will be funded by Zone D at 100% and the cost estimate is included as part of the Clinton Keith Road (From Highway 79 to Menifee Road) project. This project will also be supplemented by TUMF.
11. Clinton Keith Wildlife Overcrossing This improvement will be funded by Zone D at 100% and cost estimate is included as part of the Clinton Keith Road (From Highway 79 to Menifee Road) project. This project will also be supplemented by TUMF.

Landscaped Medians

1. Clinton Keith Road/Benton Road (SR79 to Pourroy Road) This improvement will be funded by Zone D at 100%. Tract 29214 has been conditioned to construct the median on Benton Road between Pourroy Road and Washington, and have requested an agreement for reimbursement under the current program. Therefore, the limits of the median have been reduced to reflect the construction activities performed by Tract 29214 that will occur prior to adoption of this amendment.

D. Facilities to be removed from the District:

1. Interchange of Baxter Road @ I-15 Freeway
Facility to be funded by the TUMF Program.

2. Interchange of Clinton Keith Road @ I-215 Freeway This project will be funded by the City of Murrieta and will contribute most of the needed funding not covered by the TUMF program.
3. Interchange of Bundy Canyon Road @ I-15 Freeway
4. Interchange of Los Alamos Road @ I-215 Freeway This project will be funded by TUMF and the City of Murrieta at 72.8%, and no longer needs to be included in the District.
5. Central Street
Facility to be funded by the TUMF Program.
6. Baxter Road
Facility to be funded by the TUMF Program.
7. Palomar Street
Facility to be funded by the TUMF Program.
8. Washington Street
Facility to be funded by the TUMF Program.
9. Clinton Keith Road Bridge at Murrieta Creek
Facility completed and will be removed from the District.

V. ADJUSTMENTS DUE TO INCREASES IN ESTIMATED CONSTRUCTION COST

The Transportation Department is proposing to: adjust the cost estimates for several facilities; two(2) new facilities; and remove nine(9) facilities. The increases in cost estimates are needed to reflect more precise design and engineering information. Significant increases in the cost of construction have impacted several projects since the last amendment to the District. Several programs, including TUMF, will be utilized to fund estimated project costs. Table 3 on page 8 and Exhibit C further details the proposed estimated costs of facilities compared with existing estimates.

There are two(2) new facilities that will be incorporated into this request and they are:

	NEW PROPOSED FACILITIES	Proposed RBBB Budgets	Zone
	A. Roads/Drainage Improvements		
1	Keller(Rte. 79 to Washington)	3,362,688	D
	B. Bridges		
2	La Estrella Bridge	5,000,000	A
	TOTAL	8,362,688	

These facilities have been identified by the General Plan and are needed to sustain traffic demands in this area. These facilities have not been included in the TUMF Program and will be included in the Southwest District. Zones A and D will fund these facilities.

Nine(9) facilities are proposed for removal in this request. The facilities listed below will no longer be funded by the Southwest District. These facilities are funded by the TUMF Program and additional funding from the Southwest District is not necessary at this time for construction. The Clinton Keith Bridge has been completed and facility needs to be removed from the District.

	FACILITIES TO BE REMOVED	Proposed RBBB Budgets	Current RBBB Budgets	Zone
	Name of Facility			
1	Baxter Road Interchange @ I-15	DELETE	9,759,960	A
2	Bundy Canyon Interchange @ I -15	DELETE	9,759,960	A
3	Clinton Keith Road Interchange @I-215	DELETE	2,293,591	D
4	Los Alamos Road Interchange	DELETE	2,654,709	D
5	Central Street(Palomar to I-15)	DELETE	1,206,900	A
6	Baxter Road(I-15 to 6,000' E'ly of I-15)	DELETE	1,830,600	A
7	Palomar(Mission Tr to City of Murrieta)	DELETE	3,982,770	A
8	Washington(City of Murrieta to Keller Rd)	DELETE	13,891,150	D
9	Clinton Keith Bridge(Completed)	DELETE	294,840	A, C
	TOTAL		45,674,480	

Overall, the requested changes in this report summarized will increase costs to the District. The addition of two(2) new facilities, and deletion of nine(9) facilities do not increase costs to the District. The impacts to this District are mostly recognized from the expansion of funding of lanes on two(2) included facilities, the adjustment of cost estimates for existing facilities, and the developable units left for development. August 2002 was the last time costs were updated and since then, costs for construction materials have increased significantly. Overall, the addition of two(2) new facilities, removal of nine(9) facilities, and increases to construction materials for included facilities, along with using received revenue to help offset costs, results in a 29.10 % decrease to the overall District. The District's cost has decreased by approximately \$26 million in aggregate costs to Zones A, C, and D cumulatively. The amount of revenue received by the District since the last amendment in 2002 has also significantly increased by approximately \$23.7 million. This revenue received from paid fees is used to help offset some of the costs as detailed in Table 3 on page 8.

VI. RESULTING ADJUSTMENTS TO THE FEE SCHEDULE

The fee schedule for the Southwest District is calculated by distributing the estimated construction cost of all facilities identified for the Zone among all land use designations in the Zone after converting those land use designations into equivalent dwelling units ("EDUs"). For "Residential" the conversion is based on a ratio of 1 EDU per residential dwelling unit, for "Commercial" the ratio is 15 EDUs per acre, for "Office Commercial" a ratio of 10 EDUs per acre is used, and for "Industrial" and "Airport" the ratio is 6 EDUs per acre. These ratios represent the respective traffic generation impacts associated with each land use designation utilized in the Southwest District Traffic Model Documentation. The mitigation of traffic impacts provided by the Southwest District establishes the benefit to be received by each parcel or lot and is equitably distributed to each land use designation by the above EDU ratios.

The following lists the proposed and original fees for each land use designation in the Southwest District through the end of March 31, 2007:

ZONE A

EXISTING ZONE A FEES					
	Qty.		Fee		Revenue
Residential	10,359	/DU	1,743	/DU	\$18,055,740
Commercial	658	/AC	26,145	/AC	\$17,203,410
Office Comm.	83	/AC	17,430	/AC	\$1,447,000
Industrial	436	/AC	10,458	/AC	\$4,560,000
TOTAL					\$41,266,150
PROPOSED ZONE A FEES					
	Qty.		Prop. Fee		Prop. Rev.
Residential	8,241	/DU	1,447	/DU	\$11,924,730
Commercial	563	/AC	21,705	/AC	\$12,219,920
Office Comm.	92	/AC	14,470	/AC	\$1,447,000
Industrial	369	/AC	8,682	/AC	\$4,560,000
TOTAL					\$30,151,650
					NEEDED REVENUES
					\$30,157,081

ZONE C

EXISTING ZONE C FEES					
	Qty.		Fee		Revenue
Residential	2,813	/DU	487	/DU	\$1,369,931
Commercial	0	/AC	7,305	/AC	\$0
Office Comm.	0	/AC	4,870	/AC	\$0
Industrial	0	/AC	2,922	/AC	\$0
TOTAL					\$1,369,931
PROPOSED ZONE C FEES					
	Qty.		Prop. Fee		Prop. Rev.
Residential	1,711	/DU	1,284	/DU	\$2,196,924
Commercial	0	/AC	19,260	/AC	\$0
Office Comm.	0	/AC	12,840	/AC	\$0
Industrial	0	/AC	7,704	/AC	\$0
TOTAL					\$2,196,924
			NEEDED REVENUES		\$2,198,622

ZONE D

EXISTING ZONE D FEES					
	Qty.		Fee		Revenue
Residential	10,969	/DU	2,215	/DU	\$24,296,335
Commercial	283	/AC	33,225	/AC	\$9,402,675
Office Comm.	139	/AC	22,150	/AC	\$3,078,850
Industrial	753	/AC	13,290	/AC	\$10,007,370
Airport	9	/AC	13,290	/AC	\$119,610
TOTAL					\$46,904,840
PROPOSED ZONE D FEES					
	Qty.		Prop. Fee		Prop. Rev.
Residential	5,462	/DU	2,197	/DU	\$12,000,014
Commercial	287	/AC	32,955	/AC	\$9,448,528
Office Comm.	110	/AC	21,970	/AC	\$2,416,700
Industrial	543	/AC	13,182	/AC	\$7,157,826
Airport	6	/AC	13,182	/AC	\$75,533
TOTAL					\$31,098,601
			NEEDED REVENUES		\$31,110,433

This request removes the program overlaps between the TUMF and the District, eliminating the need to provide credits. When combined, fees paid for TUMF and the District overall increase for developing properties in all of the Zones in the Southwest District. Table 5 provides a comparison of what property owners will expect to pay for TUMF and District fees combined. The Table compares TUMF fees with Existing District fees and with the new proposed District fees. Exhibit E also provides a more detailed comparison between the TUMF Program and the Southwest District.

Table 4-TUMF and District Fee Rate Comparisons

ZONE A									
	Existing	Proposed		Fee Rate	Variance	RBB & TUMF		RBB & TUMF	
	Fee	Fee		Incr/Decr	%	Current	Proposed	Comparison	Comparison %
Residential	1,743	1,447	/DU	-296	-17%	10,155	11,493	1,338	13.18%
Commercial	26,145	21,705	/AC	-4,440	-17%	96,838	108,738	11,900	12.29%
Office Comm.	17,430	14,470	/AC	-2,960	-17%	56,283	64,216	7,933	14.09%
Industrial	10,458	8,682	/AC	-1,776	-17%	27,967	32,727	4,760	17.02%
ZONE C									
	Existing	Proposed		Fee Rate	Variance	RBB & TUMF		RBB & TUMF	
	Fee	Fee		Incr/Decr	%	Current	Proposed	Comparison	Comparison %
Residential	487	1,284	/DU	\$797	164%	10,474	11,330	856	8.17%
Commercial	7,305	19,260	/AC	\$11,955	164%	93,748	106,293	12,545	13.38%
Office Comm.	4,870	12,840	/AC	\$7,970	164%	54,223	62,586	8,363	15.42%
Industrial	2,922	7,704	/AC	\$4,782	164%	26,731	31,749	5,018	18.77%
ZONE D									
	Existing	Proposed		Fee Rate	Variance	RBB & TUMF		RBB & TUMF	
	Fee	Fee		Incr/Decr	%	Current	Proposed	Comparison	Comparison %
Residential	2,215	2,197	/DU	-\$18	-1%	10,505	12,243	1,738	16.54%
Commercial	33,225	32,955	/AC	-\$270	-1%	102,698	119,988	17,290	16.84%
Office Comm.	22,150	21,970	/AC	-\$180	-1%	60,189	71,716	11,527	19.15%
Industrial	13,290	13,182	/AC	-\$108	-1%	30,311	37,227	6,916	22.82
Airport	13,290	13,182	/AC	-\$108	-1%	13,290	13,182	-108	-0.81%
				TOTAL					

Zone D is most affected by the requested changes herein. In comparison to other zones, Zone D burdens higher overall costs. These increases can be attributed to: expansion of funding to 6 lanes on Winchester, addition of one(1) new facility, significant cost increases for included facilities such as Clinton Keith Road, and less developable units than other zones of which cost can be spread to help reduce these increased affects. Several residential units were lost to the annexation that was completed by the City of Murrieta and Temecula since the last update to the District in 2002. The Table 5 below summarizes zone costs and developable units left in each zone through the end of March 31, 2007. Additionally, the last column compares cost and developable units between Zone D and Zone A. Zone C funds two(2) facilities and

its associated costs and development units is relatively small. Therefore, Zone C does not compare well with Zone D or Zone A.

Table 5

Column #	(1)	(2)	(2-1)
			Zone D incre/decr
Costs by Zone	Zone A	Zone D	Comparison to Zone A
Total Cost	41,892,500	45,792,939	3,900,439
Revenues Received	11,735,419	14,682,506	2,947,087
Adjust Total Cost	30,157,081	31,110,433	953,352
Projected Developable Units			
Residential Dus (Dwelling Units)	8,241	5,462	-2,779
Commercial (per acre)	563	287	-276
Office Comm. (per acre)	92	110	18
Industrial (per acre)	369	543	174
Airport (per acre)		6	6

VII. FINDINGS

1. By the adoption of Resolution No. 2001-317 on September 25, 2001, the Board reestablished the Southwest District, which consists of three Zones, and established a fee schedule for each Zone that equitably apportions the estimated construction costs for each of the identified facilities whose construction is identified in the Circulation Element of the County's Comprehensive General Plan and is required to mitigate the traffic circulation impacts resulting from the projected development within each Zone.
2. Environmental Assessment No. 41164 has been prepared to evaluate the proposed changes as described in this Report.
3. All existing and proposed identified facilities for the Southwest District are designated in the Circulation Element of the Riverside County Integrated Project to be constructed to mitigate traffic impacts.
4. The proposed fee schedule for each zone of the Southwest District provides an equitable apportionment of the estimated construction costs of the identified facilities needed to meet the increased traffic anticipated by the current development projections.
5. The Transportation Department has properly adjusted the land use projections for each land use designation to reflect the adoption of the new

Southwest Area Land Use Plan for the Riverside County Integrated Project, Highway 79 Policy Area, and annexations.

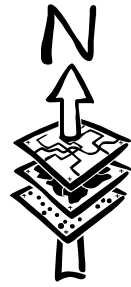
6. The Transportation Department has properly adjusted the estimated construction cost for each facility to reflect the addition and/or removal, or the modification of several facilities within the District.
7. The recommended change in fee schedules for this District will provide adequate financing to construct the proposed regional transportation improvement projects. On some facilities the District funding is needed to fully construct facilities beyond the funds identified in the TUMF Program.

VIII. RECOMMENDATION

The Transportation Department recommends the adoption of the proposed Amendment No. 2 to the Southwest Area Road and Bridge Benefit District to revise the covered facilities and total estimated costs, expand funding for two(2) included facilities, and adjust the fee schedules in Zones A, C, and D of the Southwest District as indicated in this Report.

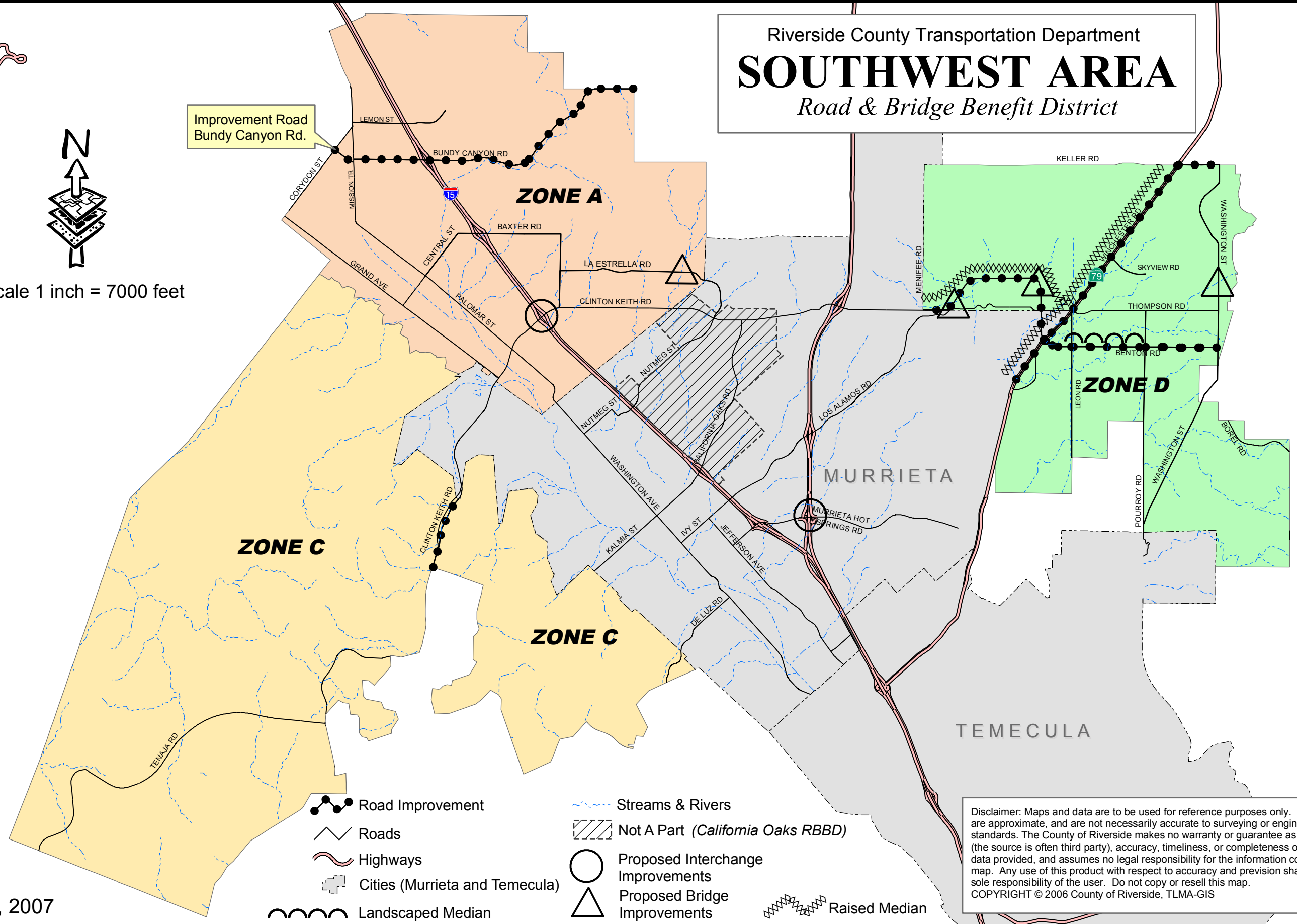
EXHIBIT A

Riverside County Transportation Department
SOUTHWEST AREA
Road & Bridge Benefit District



Scale 1 inch = 7000 feet

Improvement Road
Bundy Canyon Rd.

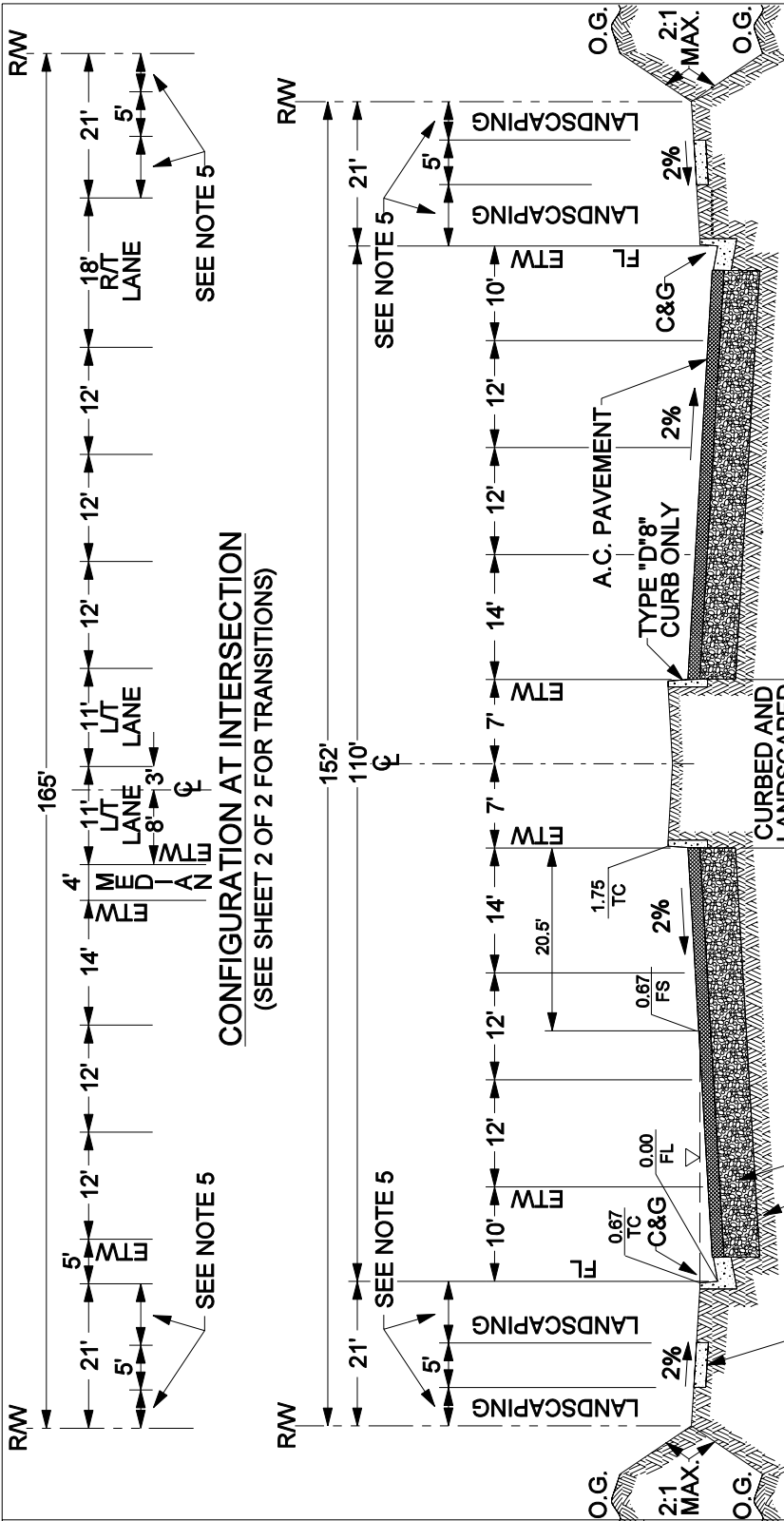


- Road Improvement
- Roads
- Highways
- Cities (Murrieta and Temecula)
- Landscaped Median
- Streams & Rivers
- Not A Part (*California Oaks RBB*)
- Proposed Interchange Improvements
- Proposed Bridge Improvements
- Raised Median

Disclaimer: Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and prevision shall be the sole responsibility of the user. Do not copy or resell this map.
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June 11, 2007

EXHIBIT B



CONFIGURATION AT INTERSECTION
(SEE SHEET 2 OF 2 FOR TRANSITIONS)

GENERAL CONFIGURATION

NOT TO SCALE

NOTE:

1. THICKNESS OF A.B. AND A.C. TO BE DETERMINED BY SOIL TEST AT TIME OF GRADING OPERATIONS.
2. MINIMUM PAVING THICKNESS PER SPECIFICATIONS SECTION 8.07 OF THIS ORDINANCE.
3. SEE STANDARD NO. 113 FOR LANDSCAPED MEDIAN REQUIREMENTS, INCLUDING MEDIAN CURB.
4. CURB AND GUTTER TO BE TYPE "A-8" (STD. NO. 201). 6" CURB IS NOT ALLOWED.
5. DISTANCE FROM SIDEWALK TO R/W, AND TO FLOW LINE, VARIES. SEE STANDARD NO. 404 FOR DETAILS OF MEANDERING SIDEWALK.
6. MAX. WATER DEPTH FOR Q_{10} IS 0.67' TO PROVIDE MIN. 12' DRY LANE.
7. FOR UNDERGROUND UTILITY LOCATIONS SEE STD. NO. 817, SHT. 2 OF 2.

ABBREVIATIONS

- A.B. = AGGREGATE BASE
- A.C. = ASPHALT CONCRETE
- C&G = CURB AND GUTTER
- ETW = EDGE OF TRAVELED WAY
- FL = FLOWLINE
- L/T = LEFT TURN
- O.G. = ORIGINAL GROUND
- R/T = RIGHT TURN
- RW = RIGHT OF WAY
- TC = TOP OF CURB
- FS = FINISHED SURFACE

APPROVED BY:

DATE: 11/15/04

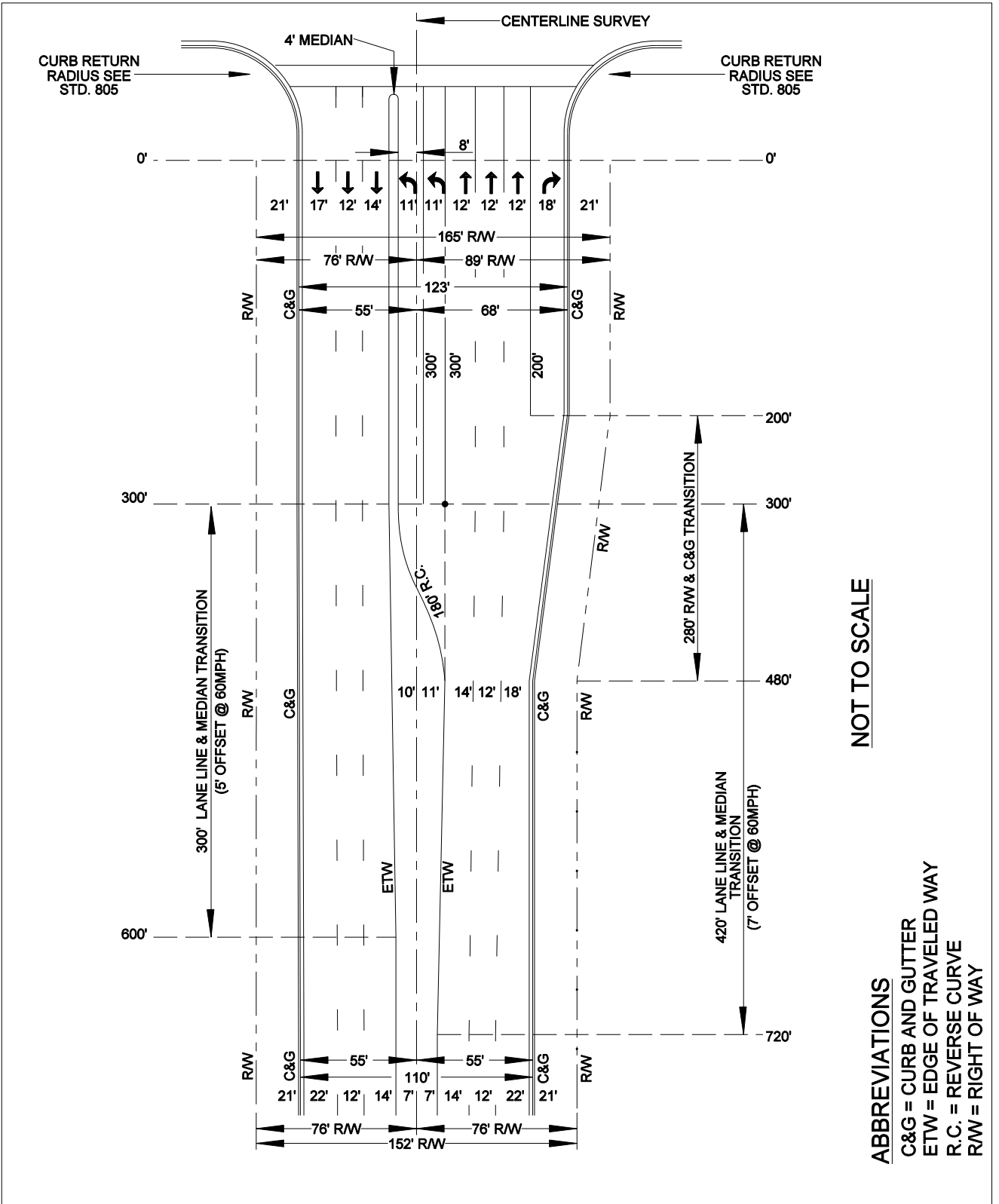
DIRECTOR OF TRANSPORTATION
GEORGE A. JOHNSON, RCE 42328

COUNTY OF RIVERSIDE

**URBAN ARTERIAL
HIGHWAY
(152' R/W)**

STANDARD NO. 91 (1 OF 2)

REVISIONS	REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE
	1				4			
	2				5			
	3				6			



APPROVED BY: _____

DATE: 11/15/04

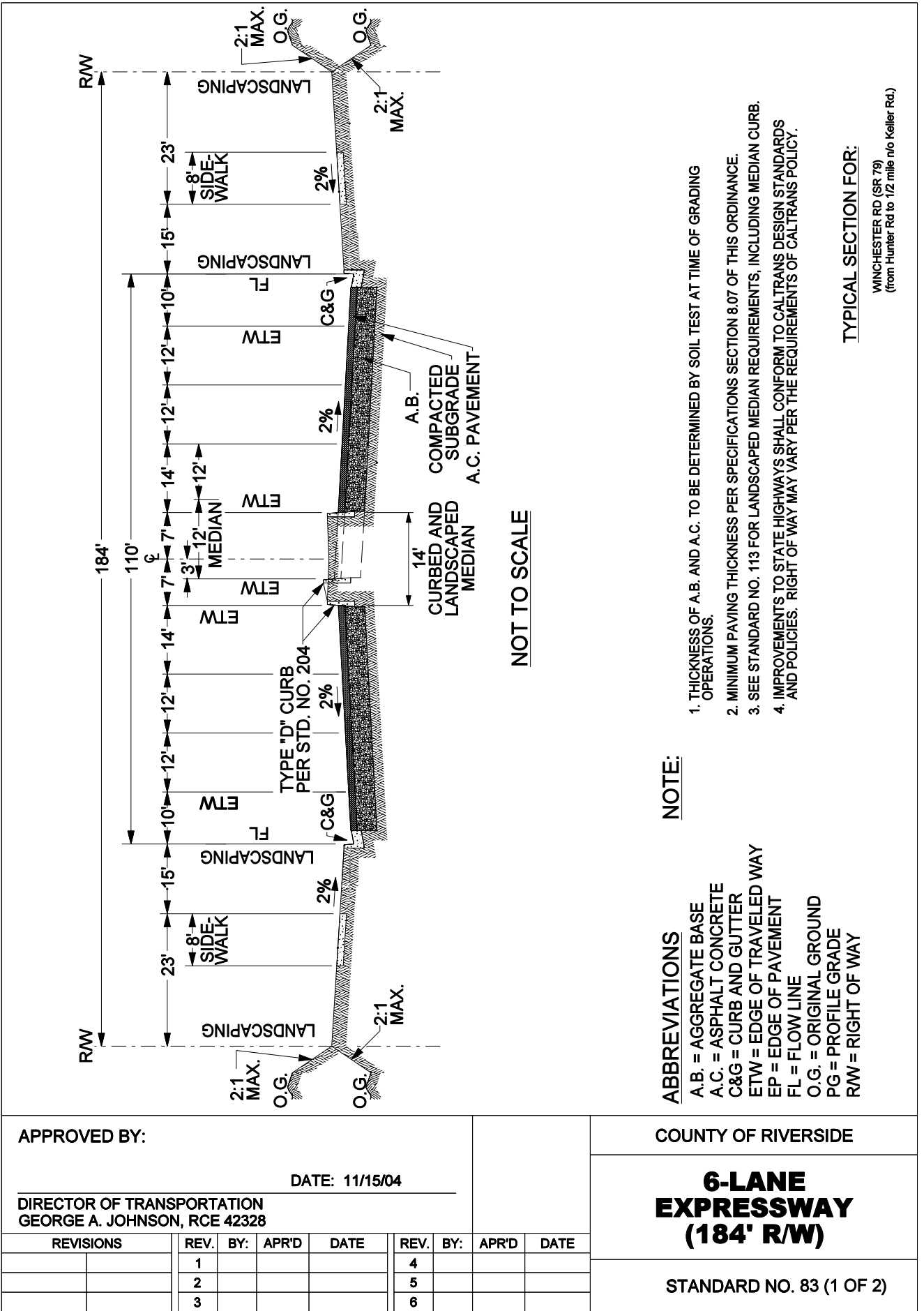
DIRECTOR OF TRANSPORTATION
 GEORGE A. JOHNSON, RCE 42328

REVISIONS	REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE
	1				4			
	2				5			
	3				6			

COUNTY OF RIVERSIDE

**URBAN ARTERIAL
 HIGHWAY
 (152' R/W)**

STANDARD NO. 91 (2 OF 2)



NOT TO SCALE

NOTE:

1. THICKNESS OF A.B. AND A.C. TO BE DETERMINED BY SOIL TEST AT TIME OF GRADING OPERATIONS.
2. MINIMUM PAVING THICKNESS PER SPECIFICATIONS SECTION 8.07 OF THIS ORDINANCE.
3. SEE STANDARD NO. 113 FOR LANDSCAPED MEDIAN REQUIREMENTS, INCLUDING MEDIAN CURB.
4. IMPROVEMENTS TO STATE HIGHWAYS SHALL CONFORM TO CALTRANS DESIGN STANDARDS AND POLICIES. RIGHT OF WAY MAY VARY PER THE REQUIREMENTS OF CALTRANS POLICY.

ABBREVIATIONS

- A.B. = AGGREGATE BASE
- A.C. = ASPHALT CONCRETE
- C&G = CURB AND GUTTER
- ETW = EDGE OF TRAVELED WAY
- EP = EDGE OF PAVEMENT
- FL = FLOW LINE
- O.G. = ORIGINAL GROUND
- PG = PROFILE GRADE
- RW = RIGHT OF WAY

TYPICAL SECTION FOR:

WINCHESTER RD (SR 79)
(from Hunter Rd to 1/2 mile n/o Keller Rd.)

APPROVED BY:

DATE: 11/15/04

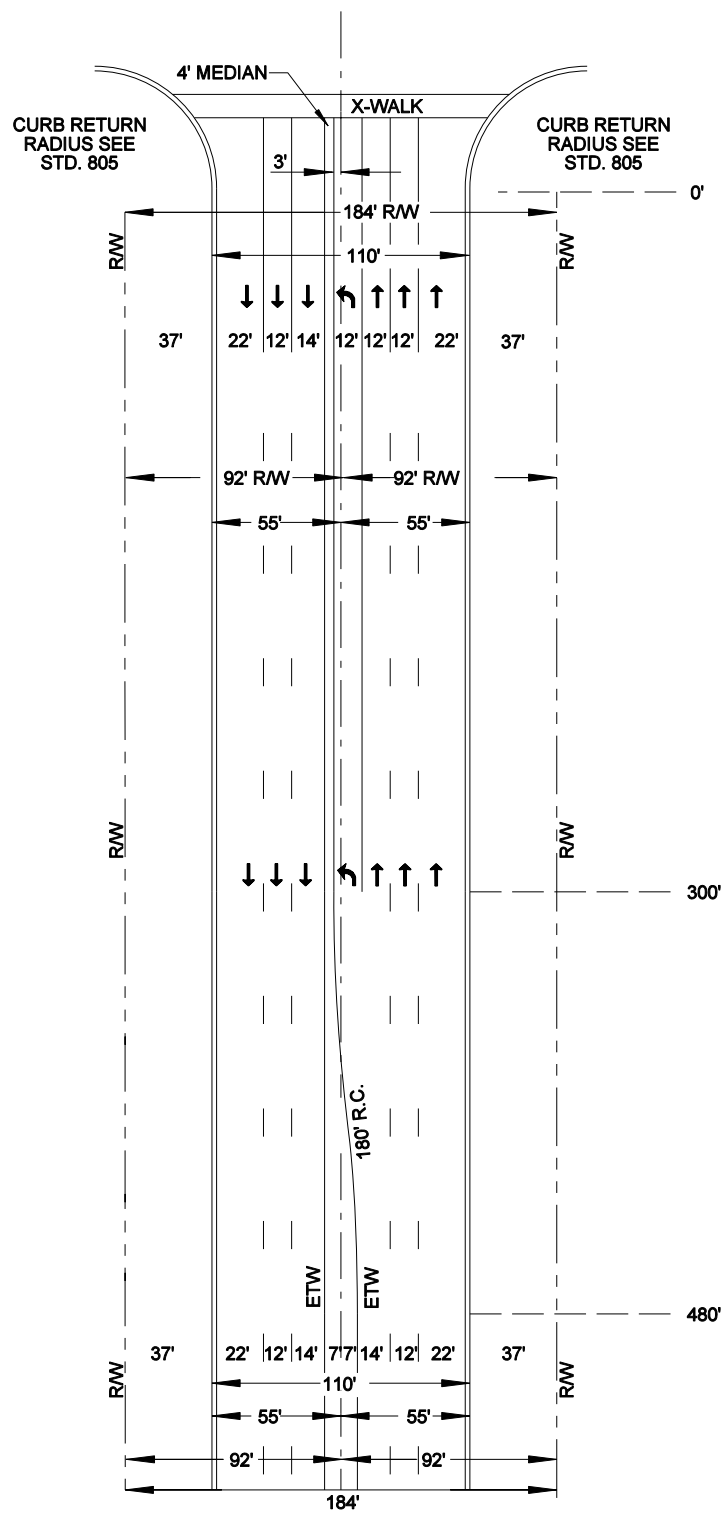
DIRECTOR OF TRANSPORTATION
GEORGE A. JOHNSON, RCE 42328

REVISIONS	REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE
	1				4			
	2				5			
	3				6			

COUNTY OF RIVERSIDE

6-LANE EXPRESSWAY (184' R/W)

STANDARD NO. 83 (1 OF 2)



NOT TO SCALE

ABBREVIATIONS
 ETW = EDGE OF TRAVELED WAY
 RW = RIGHT OF WAY

APPROVED BY: _____

DATE: 11/15/04

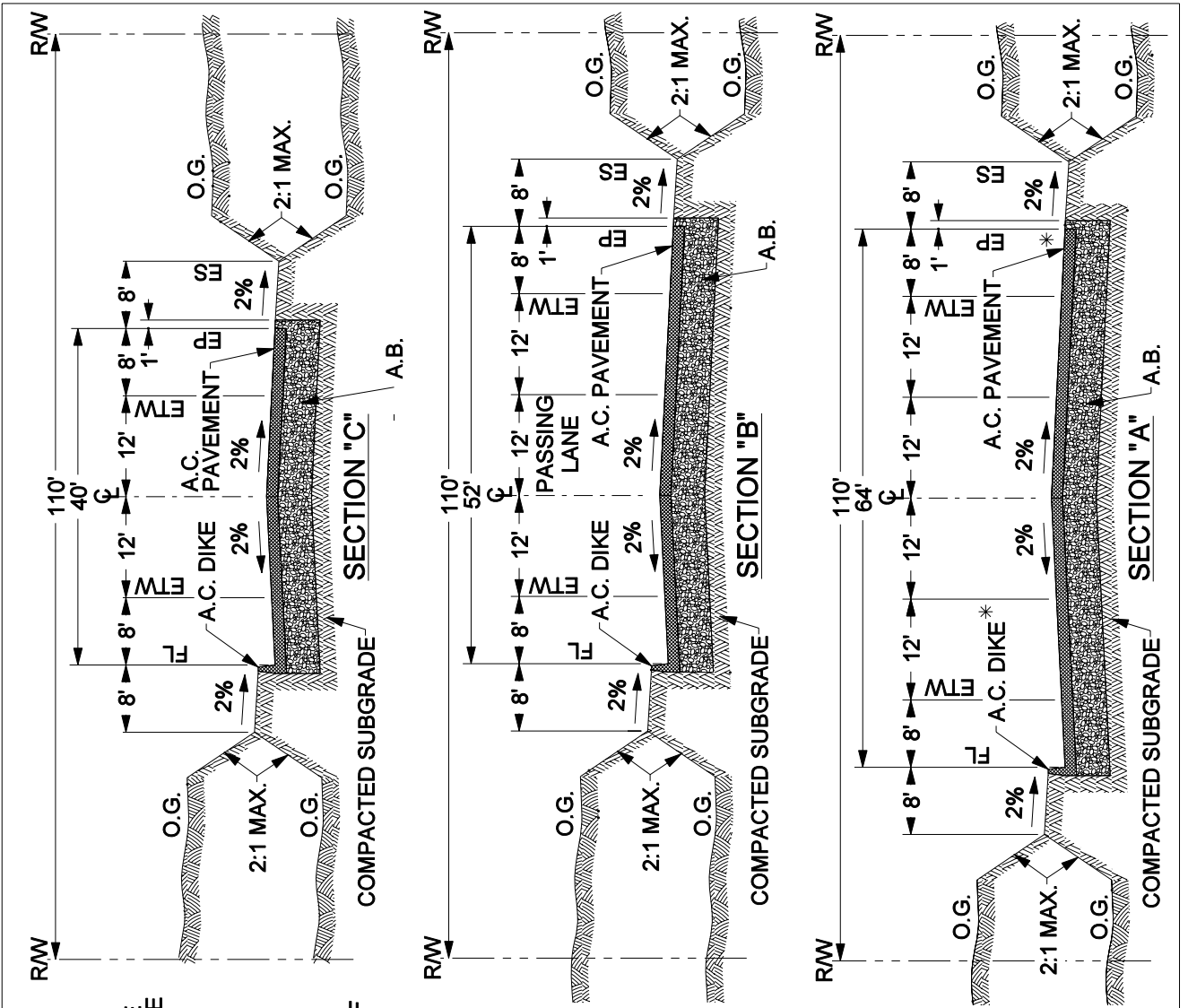
DIRECTOR OF TRANSPORTATION
 GEORGE A. JOHNSON, RCE 42328

COUNTY OF RIVERSIDE

**6-LANE EXPRESSWAY
 (184' R/W)**

REVISIONS	REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE
	1				4			
	2				5			
	3				6			

STANDARD NO. 83 (2 OF 2)



NOTE:

1. THICKNESS OF A.B. AND A.C. TO BE DETERMINED BY SOIL TEST AT TIME OF GRADING OPERATIONS.
2. MINIMUM PAVING THICKNESS PER SPECIFICATIONS SECTION 8.07 OF THIS ORDINANCE.
3. SEE STANDARD NO. 212 FOR A.C. DIKE DETAIL.
- *4. CONCRETE CURB, GUTTER, AND SIDEWALK MAY BE REQUIRED AS DETERMINED BY THE DIRECTOR OF TRANSPORTATION.

NOT TO SCALE

- A.B. = AGGREGATE BASE
- A.C. = ASPHALT CONCRETE
- EP = EDGE OF PAVEMENT
- ES = EDGE OF SHOULDER
- ETW = EDGE OF TRAVELED WAY
- FL = FLOW LINE
- O.G. = ORIGINAL GROUND
- RW = RIGHT OF WAY

APPROVED BY:

DATE: 11/15/04

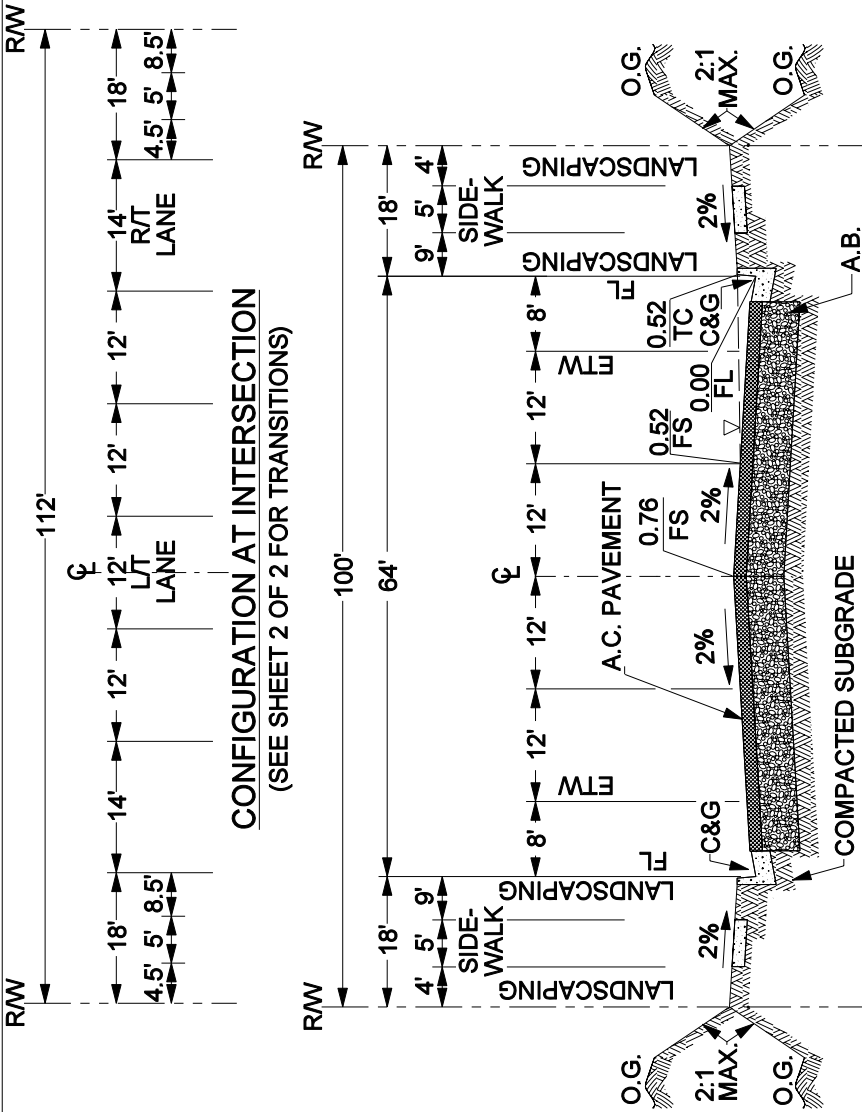
DIRECTOR OF TRANSPORTATION
GEORGE A. JOHNSON, RCE 42328

REVISIONS	REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE
	1				4			
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	3				6			

COUNTY OF RIVERSIDE

**MOUNTAIN ARTERIAL
HIGHWAY
(110' R/W)**

STANDARD NO. 95



CONFIGURATION AT INTERSECTION
(SEE SHEET 2 OF 2 FOR TRANSITIONS)

GENERAL CONFIGURATION
NOT TO SCALE

NOTE:

1. THICKNESS OF A.B. AND A.C. TO BE DETERMINED BY SOIL TEST AT TIME OF GRADING OPERATIONS.
2. MINIMUM PAVING THICKNESS PER SPECIFICATIONS SECTION 8.07 OF THIS ORDINANCE.
3. CURB AND GUTTER TO BE TYPE "A-6" (STD. NO. 200). 8" CURB IS NOT ALLOWED.
4. MAX. WATER DEPTH FOR Q_{10} IS 0.52' TO PROVIDE MIN. 12" DRY LANE.
5. FOR UNDERGROUND UTILITY LOCATIONS SEE STD. NO. 817, SHT. 2 OF 2.

ABBREVIATIONS

- A.B. = AGGREGATE BASE
- A.C. = ASPHALT CONCRETE
- C&G = CURB AND GUTTER
- ETW = EDGE OF TRAVELED WAY
- FL = FLOW LINE
- L/T = LEFT TURN
- O.G. = ORIGINAL GROUND
- R/T = RIGHT TURN
- R/W = RIGHT OF WAY
- TC = TOP OF CURB
- FS = FINISHED SURFACE

APPROVED BY:

DATE: 11/15/04

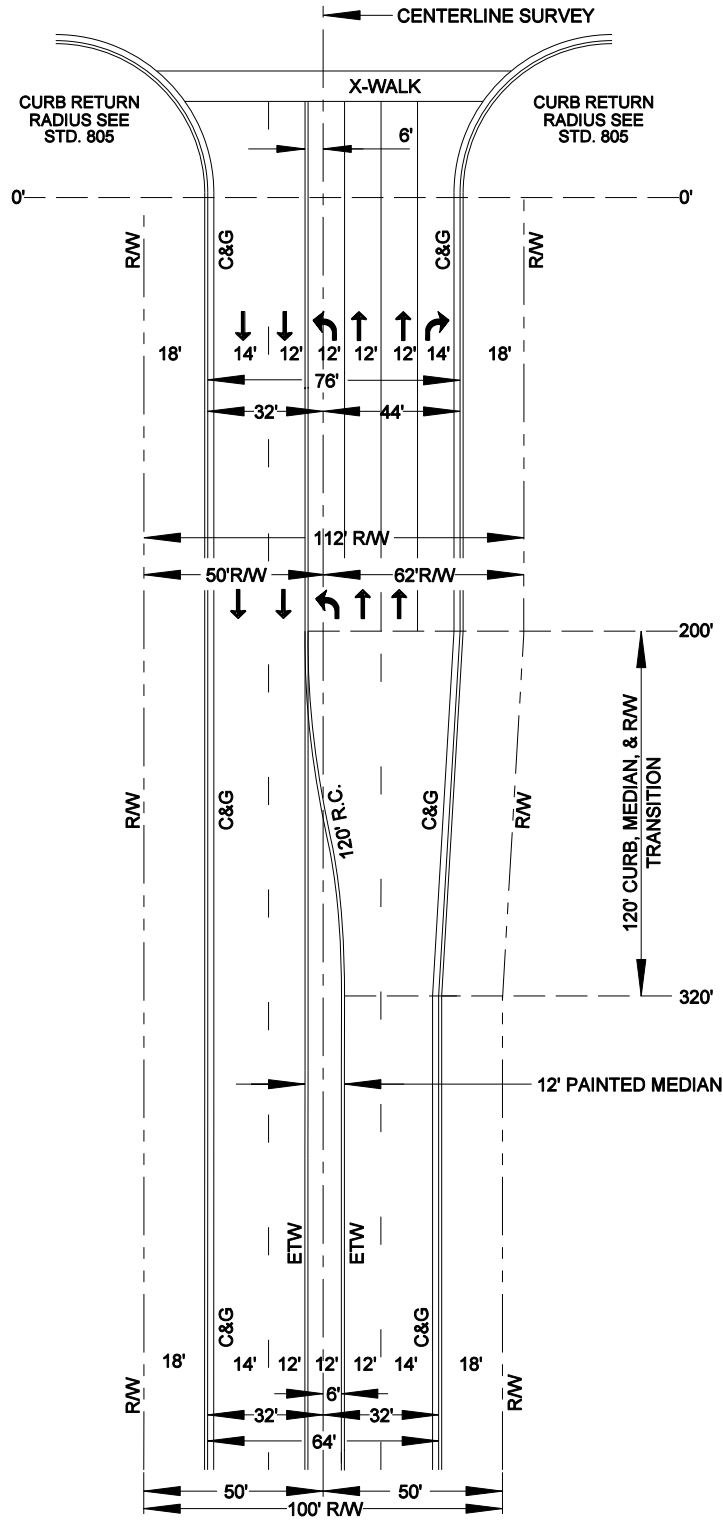
DIRECTOR OF TRANSPORTATION
GEORGE A. JOHNSON, RCE 42328

REVISIONS	REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE
	1				4			
	2				5			
	3				6			

COUNTY OF RIVERSIDE

**SECONDARY
HIGHWAY
(100' R/W)**

STANDARD NO. 94 (1 OF 2)



NOT TO SCALE

ABBREVIATIONS
 C&G = CURB AND GUTTER
 ETW = EDGE OF TRAVELED WAY
 R.C. = REVERSE CURVE
 R/W = RIGHT OF WAY

APPROVED BY:

DATE: 11/15/04

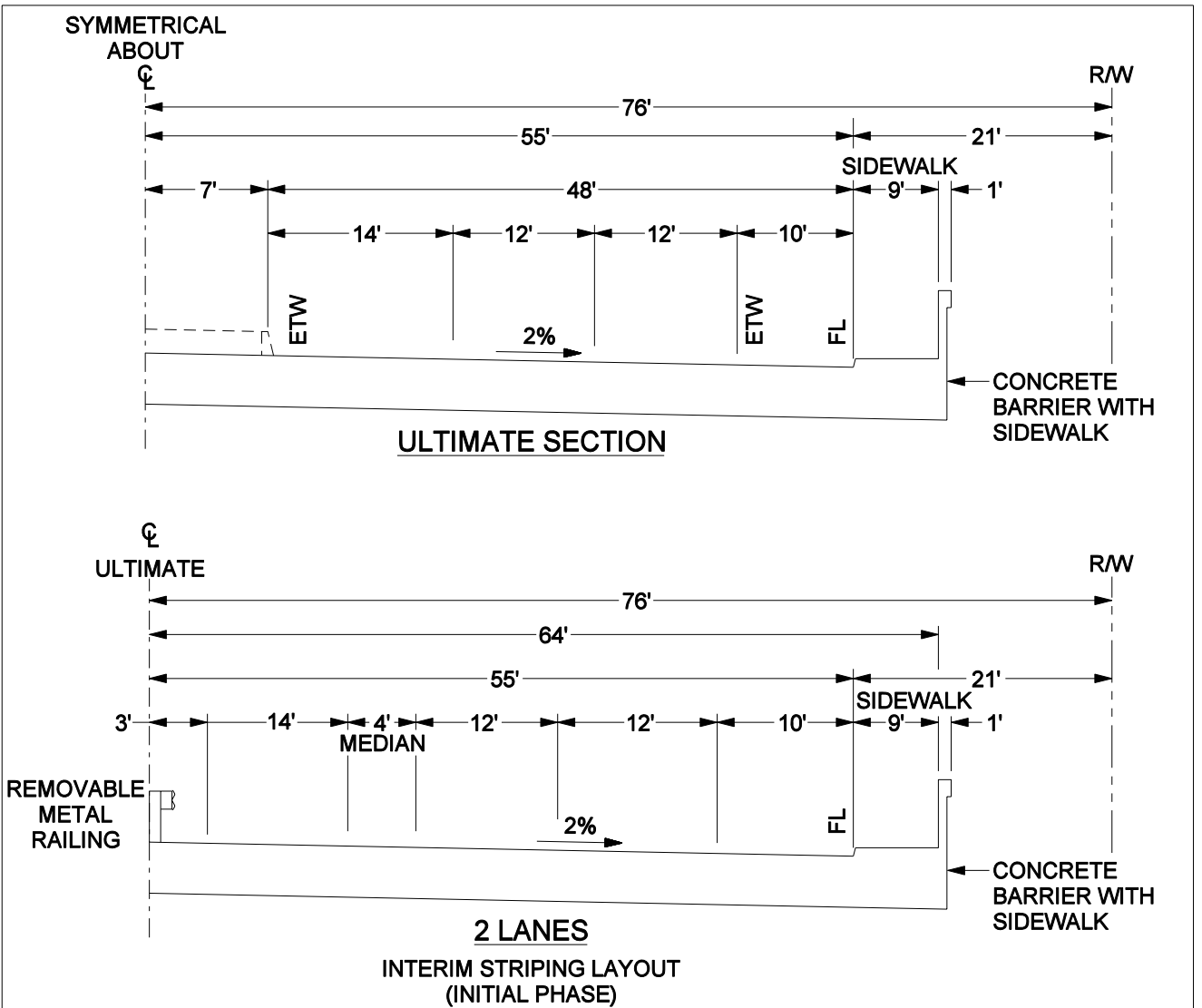
DIRECTOR OF TRANSPORTATION
 GEORGE A. JOHNSON, RCE 42328

COUNTY OF RIVERSIDE

**SECONDARY HIGHWAY
 (100' R/W)**

REVISIONS		REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE
2-04		1				4			
		2				5			
		3				6			

STANDARD NO. 94 (2 OF 2)



NOTE:

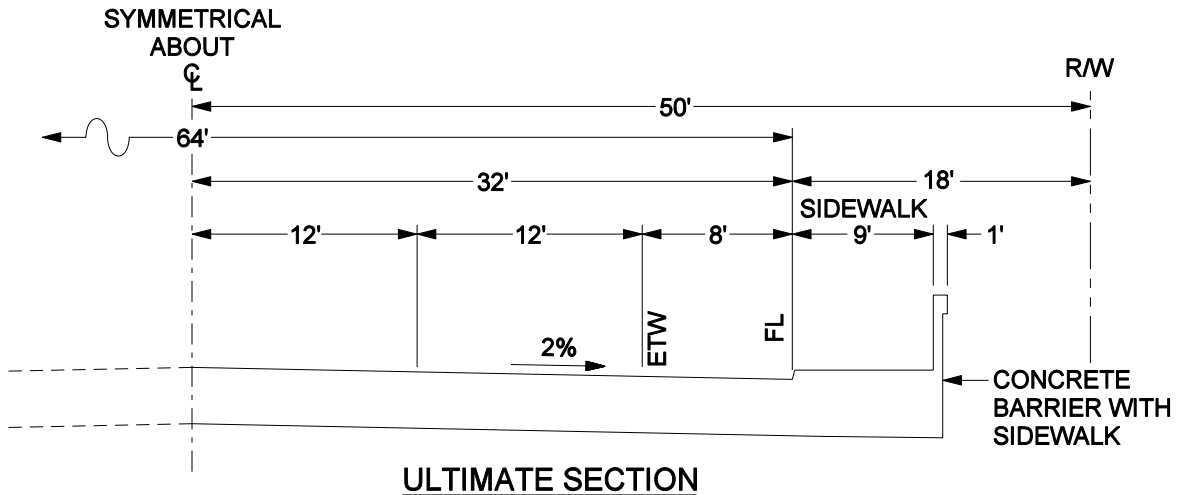
1. ULTIMATE TYPICAL BRIDGE SECTIONS TO BE CONSISTENT WITH THE APPROACH ROAD SECTION AS APPROVED BY THE TRANSPORTATION DEPARTMENT.
2. BRIDGE TYPE TO BE APPROVED BY THE TRANSPORTATION DEPARTMENT.
3. CURBED MEDIAN TO BE USED ONLY WHEN APPROACHING HIGHWAY HAS A RAISED MEDIAN. MEDIAN WIDTH MAY VARY WITH RAISED CURB.
4. MEDIAN WIDTH SUBJECT TO VARIATION DEPENDING ON INTERSECTION PROXIMITY.
5. RAISED SIDEWALK TO BE PROVIDED. NON-SIDEWALK CONCRETE BARRIER TO BE USED ONLY IF APPROVED BY THE DIRECTOR OF TRANSPORTATION.
6. CONCRETE BARRIER SHALL BE TO CALTRANS STANDARDS OR AS APPROVED BY THE DIRECTOR OF TRANSPORTATION.

ABBREVIATIONS

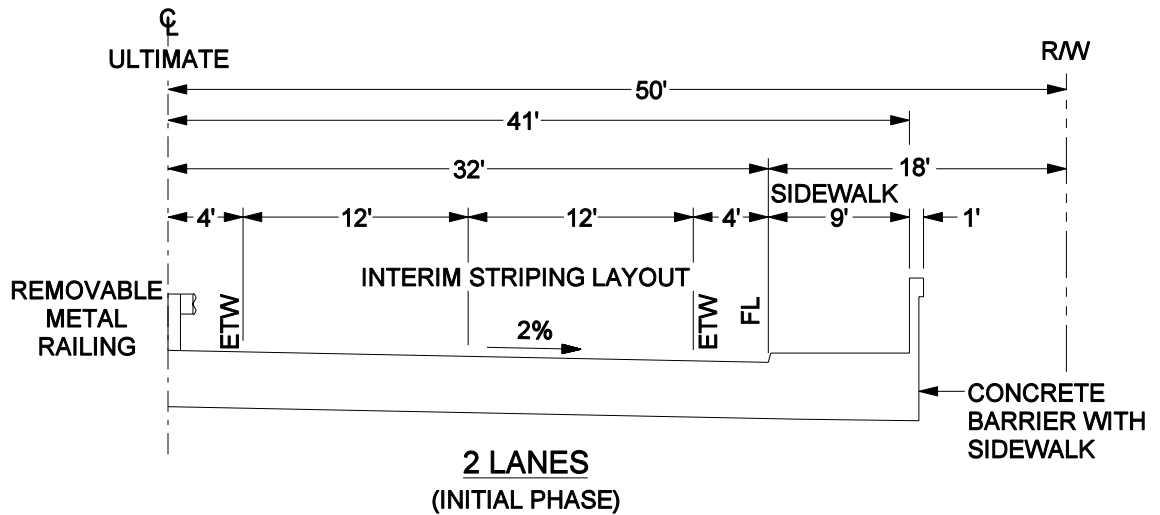
ETW = EDGE OF TRAVELED WAY
 FL = FLOW LINE

NOT TO SCALE

APPROVED BY: _____ DATE: 11/15/04 DIRECTOR OF TRANSPORTATION GEORGE A. JOHNSON, RCE 42328					COUNTY OF RIVERSIDE URBAN ARTERIAL HIGHWAY BRIDGE STANDARD NO. 116				
REVISIONS	REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE	
12-97, 11-04	1				4				
	2				5				
	3				6				



ULTIMATE SECTION



**2 LANES
(INITIAL PHASE)**

NOTE:

1. ULTIMATE TYPICAL BRIDGE SECTIONS TO BE CONSISTENT WITH THE APPROACH ROAD SECTION AS APPROVED BY THE TRANSPORTATION DEPARTMENT.
2. BRIDGE TYPE TO BE APPROVED BY THE TRANSPORTATION DEPARTMENT.
3. RAISED SIDEWALK TO BE PROVIDED. NON-SIDEWALK CONCRETE BARRIER TO BE USED ONLY IF APPROVED BY THE DIRECTOR OF TRANSPORTATION.
4. CONCRETE BARRIER SHALL BE TO CALTRANS STANDARDS OR AS APPROVED BY THE DIRECTOR OF TRANSPORTATION.

ABBREVIATIONS

ETW = EDGE OF TRAVELED WAY
FL = FLOW LINE

NOT TO SCALE

APPROVED BY:					DATE: 11/15/04					COUNTY OF RIVERSIDE			
DIRECTOR OF TRANSPORTATION GEORGE A. JOHNSON, RCE 42328									SECONDARY HIGHWAY BRIDGE				
REVISIONS		REV.	BY:	APR'D	DATE	REV.	BY:	APR'D	DATE	STANDARD NO. 119			
		1				4							
		2				5							
		3				6							

EXHIBIT C

SOUTHWEST FACILITIES SUMMARY

ZONE A					
FACILITIES		PROPOSED	CURRENT	VARIANCE	% VARIANCE
A. Interchanges @ I-15					
1	Baxter Road Interchange @ I-15	Delete	\$ 9,759,960	\$ (9,759,960)	-100%
2	Bundy Canyon Road Interchange @ I-15	Delete	\$ 9,759,960	\$ (9,759,960)	-100%
3	Clinton Keith Road Interchange @ I-15	\$ 15,892,500	\$ 6,470,854	\$ 9,421,646	146%
B. Roads					
4	Bundy Canyon Road(Mission Trail to Sunset Ave.)	\$ 20,000,000	\$ 8,077,413	\$ 11,922,587	148%
5	Bundy Canyon(Mission Trail to Corydon)	\$ 1,000,000	\$ 1,182,912	\$ (182,912)	-15%
6	Central Street (Palomar to I-15)	Delete	\$ 1,206,900	\$ (1,206,900)	-100%
7	Baxter Road (I-15 to 6,000' E'ly of I-15)	Delete	\$ 1,830,600	\$ (1,830,600)	-100%
8	Palomar Street (Mission Tr to City of Murrieta)	Delete	\$ 3,982,770	\$ (3,982,770)	-100%
C. Bridges					
9	La Estrella Bridge	\$ 5,000,000	New	\$ 5,000,000	100%
10	Clinton Keith Rd Bridge @ Murrieta Creek (7.7%)	Delete	\$ 287,469	\$ (287,469)	-100%
ZONE A Subtotal		\$ 41,892,500	\$ 42,558,838	\$ (666,338)	-2%
To Date Revenues		\$ 11,735,419	\$ 1,309,595	\$ 10,425,824	796%
Additional Revenue Needed		\$ 30,157,081	\$ 41,249,243	\$ (11,092,162)	-27%
DETAILS					
A. Interchanges @ I-15					
1	Baxter Road Interchange	Delete			
2	Bundy Canyon Road Interchange	Delete			
3	Clinton Keith Road Interchange	TOTAL COST	35,000,000		
	(Zone A 97.5%, Zone C 2.5%)	City of Murrieta Share	2,000,000		
		TUMF Share	16,700,000		
		RBBB Share	16,300,000		
B. Roadway/Drainage Improvements					
4	Bundy Canyon Road (Mission Tr to Sunset Ave)	RBBB Share	13,000,000		
	Construction of 6 lanes for approximately 23,600.				
	TUMF to supplement funding for 6 lanes				
5	Bundy Canyon(Mission Trail to Corydon)	RBBB Share	1,000,000		
	Construction of 4 lanes for approximately 1900 linear feet.				
6	Central Street (Palomar St to I-15)	Delete			
	Funded by TUMF				
7	Baxter Road (I-15 to 6,000' E'ly of I-15)	Delete			
	Funded by TUMF				
8	Palomar Street (Mission Tr to City of Murrieta)	Delete			
	Funded by TUMF				
C. Bridges					
9	La Estrella Bridge	RBBB Share	5,000,000		
10	Clinton Keith Road (7.4% A, 0.2% C, 92.1% Other)	Built			
NOTES:					
Revenue collected as of 3/31/07					

SOUTHWEST FACILITIES SUMMARY

ZONE C					
FACILITIES		PROPOSED	CURRENT	VARIANCE	% VARIANCE
1	Clinton Keith Road Interchange @ I-15	407,500	165,919	241,581	146%
2	Clinton Keith Road	2,000,000	1,206,900	793,100	66%
3	Clinton Keith Rd Bridge @ Murrieta Creek (7.7%)	Delete	7,371	(7,371)	-100%
	TOTAL	2,407,500	1,380,190	1,027,310	74%
	To Date Revenues	208,878	10,720	198,158	1848%
	Additional Revenue Needed	2,198,622	1,369,470	829,152	61%
DETAILS					
1	Clinton Keith Road Interchange (Zone A 97.5%, Zone C 2.5%)	TOTAL COST	35,000,000		
		City of Murrieta Share	2,000,000		
		TUMF Share	16,700,000		
		RBBB Share	16,300,000		
2	Clinton Keith Road (from the City of Murrieta Limits to the end boundary of Zone C) Construction of 2 lanes to cover the 3rd and 4th lanes. Total length of Project is 6,000 L.F.	RBBB Share	2,000,000		
3	Clinton Keith Rd Bridge @ Murrieta Creek (7.7%) Zone A 97.5%, Zone C 2.5%	Delete			
NOTES:					
Revenue collected as of 3/31/07					

SOUTHWEST FACILITIES SUMMARY

ZONE D					
FACILITIES		PROPOSED	CURRENT	VARIANCE	% VARIANCE
A. Interchanges					
1	Clinton Keith Road Interchange @I-215	Delete	2,293,591	(2,293,591)	-100%
2	Los Alamos Road Interchange	Delete	2,654,709	(2,654,709)	-100%
3	Murrieta Hot Springs Rd @ I-215 Interchange	2,010,690	2,654,709	(644,019)	-24%
B. Road Improvements					
4	Winchester Road/State Highway 79(Auld to Keller Rd.)	10,576,000	4,682,470	5,893,530	126%
5	Clinton Keith Rd.(Menifee Rd to Hwy 79)	22,800,000	19,282,370	3,517,630	18%
6	Benton Rd.(Hwy 79 to Washington)	3,000,000	Included #5		
7	Washington Street(City of Murrieta to Keller Rd)	Delete	13,891,150	(13,891,150)	-100%
8	Keller(Rte. 79 to Washington)	3,362,688	New	3,362,688	100%
C. Bridges					
9	Clinton Keith Road Bridge @ Warm Springs Creek East	Included in #5	Included in #5		
10	Clinton Keith Road Bridge @ Warm Springs Creek West	Included in #5	Included in #5		
11	Washington St at French Valley Stream	3,000,000	3,000,000	-	0%
12	Clinton Keith Wildlife Overcrossing	Included in #5			100%
D. Landscaped Median					
15	Benton Rd. Landscaped Median(SR79 to Pourroy Road)	1,043,561	Included #6	1,043,561	
Estimated Project Costs		45,792,939	48,458,999	(5,666,060)	-12%
Revenues Collected		14,682,506	1,560,973	13,121,533	841%
Needed Revenues		31,110,433	46,898,026	(15,787,593)	-34%
DETAILS					
A. Interchange Improvements (I-215)					
1	Clinton Keith Road Interchange	Delete			
	Zone B(City of Murrieta) 76.5%, Zone D 23.5%				
2	Interchange of Los Alamos Road @ I-215	Delete			
	Zone B(City of Murrieta) 72.8%, Zone D 27.2%				
3	Murrieta Hot Springs Interchange @ 215	RBBB Share	2,010,690		
	Zone B(City of Murrieta) 72.8%, Zone D 27.2%				
B. Roadway/Drainage Improvements (100% Funding)					
4	Winchester Road/State Highway 79(Auld to Keller Rd.)	RBBB Share	10,576,000		
	Program will cover 6 lanes				
5	Clinton Keith Rd.(Menifee Rd to Hwy 79)	TOTAL COST	71,000,000		
	Construct 6 New Lanes for Clinton Keith	TUMF Share	48,200,000		
	Includes East and West Bridges @ Warm Springs Creek	RBBB Share	22,800,000		
	Approximate length is 13,100 linear feet				
6	Benton Rd.(SR79 to Washington)	RBBB Share	3,000,000		
	Reconstruct of the centermost middle 2 lanes.				
	Approximate length is 13,500 linear feet				
7	Washington Street(City of Murrieta to Keller Rd)	Delete			
8	Keller(Rte. 79 to Washington)	RBBB Share	3,362,688		
	Construction of 4 Lanes, for 3024 linear ft				
	(Approximately 7,800 ft. of which 2,400 ft is paved between 79 & Leon Rd)				

SOUTHWEST FACILITIES SUMMARY

	C. Bridge Improvements				
9	Clinton Keith Road Bridge @ Warm Springs Creek East	RBB Share	Included in #5		
10	Clinton Keith Road Bridge @ Warm Springs Creek West (TUMF funds only \$4,640,000)	RBB Share	Included in #5		
11	Washington St at French Valley Stream	RBB Share	\$ 3,000,000		
12	Clinton Keith Wildlife Overcrossing	RBB Share	Included in #5		
	D. Landscaped Median				
15	Benton Rd. Landscaped Median (SR79 to Pourroy Road) Project Length: 9,500 linear feet	RBB Share	1,043,561		
	NOTES:				
	Revenue collected as of 3/31/07				

EXHIBIT D

SOUTHWEST FACILITY SUMMARY TOTAL												
Name of Facility	Increase of Lanes	Program Lanes	Construction	Admin/Design/ Eng	Total RBBB Share	Admin Fee 5%	Adj. Total RBBB Share	Average Total	Length (Linear F)	Reimbursement Per Linear	Zone(s)	
A. Interchanges												
1 Clinton Keith Road Interchange @ I-15			11,241,379	5,058,621	16,300,000	-815,000	15,485,000	18%			A, C	
2 Murrieta Hot Springs Rd @ I-215 Interchange			1,386,683	624,007	2,010,690	-100,535	1,910,156	2%			D	
B. Roads/Drainage Improvements												
3 Bundy Canyon Road (Mission Tr to Sunset Ave)	6	1, 2, 3, 4, 5, 6	13,793,103	6,206,897	20,000,000	-1,000,000	19,000,000	22%	23,600	\$ 134	A	
4 Bundy Canyon(Mission Trail to Corydon)	4	1, 2, 3, 4	689,655	310,345	1,000,000	-50,000	950,000	1%	1,900	\$ 125	A	
5 Clinton Keith Road (City of Murrieta to Zone C Boundary)	2	3, 4	1,379,310	620,690	2,000,000	-100,000	1,900,000	2%	6,000	\$ 158	C	
6 Winchester Road/State Highway 79(Auld to Keller Rd.)	6	1, 2, 3, 4, 5, 6	7,293,793	3,282,207	10,576,000	-528,800	10,047,200	12%	19,900	\$ 84	D	
7 Clinton Keith Rd.(Menifee Rd to Hwy 79)	6	1, 2, 3, 4, 5, 6	15,724,138	7,075,862	22,800,000	-1,140,000	21,660,000	25%	13,100	Note 2	D	
8 Benton Rd.(Hwy 79 to Washington)	2	1, 2	2,068,966	931,034	3,000,000	-150,000	2,850,000	3%	13,500	\$ 106	D	
9 Keller(Rte. 79 to Washington)	4	1, 2, 3, 4	2,319,095	1,043,593	3,362,688	-168,134	3,194,554	4%	3,024	\$ 264	D	
C. Bridges												
10 La Estrella Bridge		Bridge	3,448,276	1,551,724	5,000,000	-250,000	4,750,000	6%			A	
11 Clinton Keith Road Bridge @ Warm Springs Creek East		Bridge	Included in #7					0%		Note 2	D	
12 Clinton Keith Road Bridge @ Warm Springs Creek West		Bridge	Included in #7					0%		Note 2	D	
13 Washington St at French Valley Stream		Bridge	2,068,966	931,034	3,000,000	-150,000	2,850,000	3%			D	
D. Landscaped Medians												
14 Benton Rd. Landscaped Median(SR79 to Pourroy Road)		Median	719,697	323,864	1,043,561	-52,178	991,383	1%	9,500	\$ 104	D	
TOTAL			62,133,061	27,959,877	90,092,939	-4,504,647	85,588,292	100%				
TOTAL REVENUES RECEIVED					26,626,803							
TOTAL REVENUES NEEDED					63,466,136							
NOTES:												
1 Cost assumptions for Clinton Keith(Menifee Rd to Hwy79) include Right-Of-Way acquisition.												
2 Clinton Keith Rd.(Menifee Rd to Hwy 79) will be constructed by the Department and includes funding for the East and West Bridges over Warm Springs, the Wildlife Overcrossing.												

EXHIBIT E

TUMF PROGRAM and DISTRICT FEE RATE COMPARISONS

ZONE A - SOUTHWEST																
	Existing	Proposed		Existing	Proposed		Fee Rate	Variance	Projected	Existing		RBB & TUMF		RBB & TUMF Comparison		
	Qty.	Qty.	Variance	Fee	Fee		Incr/Decr	%	Revenue	TUMF CREDIT	TUMF FEE	Current	Proposed	Variance	Variance %	
Residential	10,359	8,241	-2,118	1,743	1,447	/DU	-296	-17%	\$11,924,730	1,634	10,046	10,155	11,493	1,338	13.18%	
Commercial	658	563	-95	26,145	21,705	/AC	-4,440	-17%	\$12,219,920	16,340	87,033	96,838	108,738	11,900	12.29%	
Office Comm.	83	92	9	17,430	14,470	/AC	-2,960	-17%	\$1,447,000	10,893	49,746	56,283	64,216	7,933	14.09%	
Industrial	436	369	-67	10,458	8,682	/AC	-1,776	-17%	\$4,560,000	6,536	24,045	27,967	32,727	4,760	17.02%	
									TOTAL	\$30,151,650						
									NEEDED REVENUES	\$30,157,081						
ZONE C - SOUTHWEST																
	Existing	Proposed		Existing	Proposed		Fee Rate	Variance	Projected	Existing		RBB & TUMF		RBB & TUMF Comparison		
	Qty.	Qty.	Variance	Fee	Fee		Incr/Decr	%	Revenue	TUMF CREDIT	TUMF FEE	Current	Proposed	Variance	Variance %	
Residential	2,813	1,711	-1,102	487	1,284	/DU	\$797	164%	2,196,924	59	10,046	10,474	11,330	856	8.17%	
Commercial	0	0	0	7,305	19,260	/AC	\$11,955	164%	0	590	87,033	93,748	106,293	12,545	13.38%	
Office Comm.	0	0	0	4,870	12,840	/AC	\$7,970	164%	0	393	49,746	54,223	62,586	8,363	15.42%	
Industrial	0	0	0	2,922	7,704	/AC	\$4,782	164%	0	236	24,045	26,731	31,749	5,018	18.77%	
									TOTAL	2,196,924						
									NEEDED REVENUES	2,198,622						
ZONE D -SOUTHWEST																
	Existing	Proposed		Existing	Proposed		Fee Rate	Variance	Projected	Existing		RBB & TUMF		RBB & TUMF Comparison		
	Qty.	Qty.	Variance	Fee	Fee		Incr/Decr	%	Revenue	TUMF CREDIT	TUMF FEE	Current	Proposed	Variance	Variance %	
Residential	10,969	5,462	-5,507	2,215	2,197	/DU	-\$18	-1%	12,000,014	1,756	10,046	10,505	12,243	1,738	16.54%	
Commercial	283	287	4	33,225	32,955	/AC	-\$270	-1%	9,448,528	17,560	87,033	102,698	119,988	17,290	16.84%	
Office Comm.	139	110	-29	22,150	21,970	/AC	-\$180	-1%	2,416,700	11,707	49,746	60,189	71,716	11,527	19.15%	
Industrial	753	543	-210	13,290	13,182	/AC	-\$108	-1%	7,157,826	7,024	24,045	30,311	37,227	6,916	22.82%	
Airport	9	6	-3	13,290	13,182	/AC	-\$108	-1%	75,533	-	0	13,290	13,182	-108	-0.81%	
									TOTAL	31,098,601						
									NEEDED REVENUES	31,110,433						
NOTE																
1. Revenue collected as of 3/31/07.																
2. Land Use Projections through 3/31/07.																
3. Proposed amendment will remove TUMF Credit.																